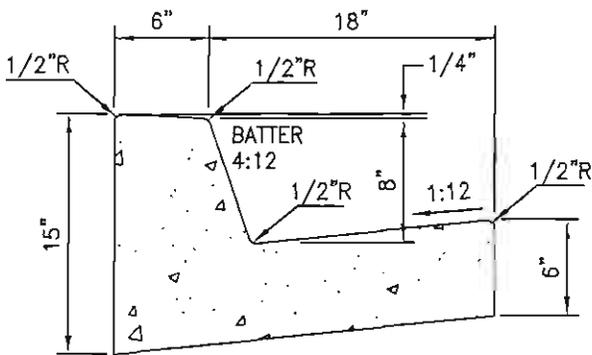


SECTION 1000
STREET IMPROVEMENTS

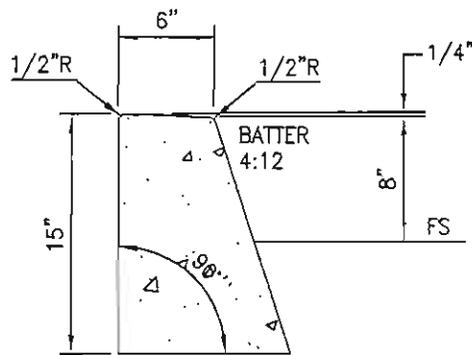
CONSTRUCTION STANDARDS

SECTION 1000 – STREET IMPROVEMENTS

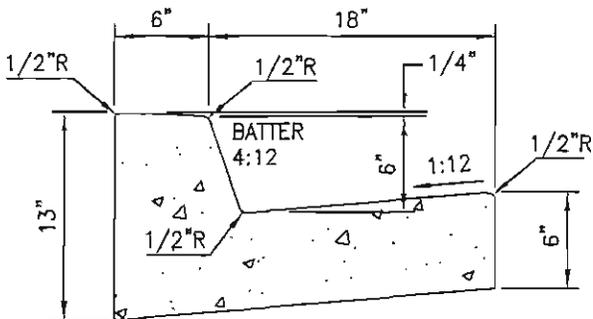
1000	Curb and Gutter
1001 (2 Sheets)	Residential/Commercial/Industrial Driveway with Curb Adjacent Sidewalks – Revised 09/26/19
1002 (2 Sheets)	Residential/Commercial/Industrial Driveway with Parkway Sidewalks
1003 (2 Sheets)	Standard Curb Return Access Ramp
1004 (2 Sheets)	Modified Curb Return Access Ramp
1005	Standard Bus Bay
1006 (2 Sheets)	Sidewalks
1007	Asphalt Concrete Dike
1008 (2 Sheets)	Standard Trench Repair – Revised 08/07/19
1009	Plate Bridging– Revised 08/07/19



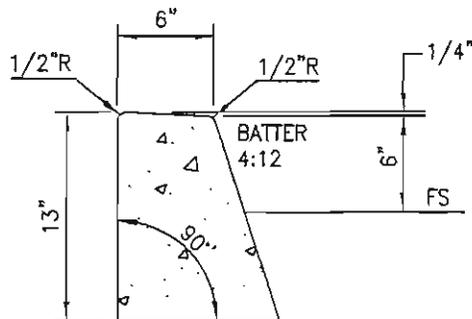
8" CURB & GUTTER
0.054 C.Y. CONC. PER L.F.



8" CURB
0.032 C.Y. CONC. PER L.F.



6" CURB & GUTTER
0.049 C.Y. CONC. PER L.F.



6" CURB
0.027 C.Y. CONC. PER L.F.

NOTES:

1. RELATIVE COMPACTION REQUIREMENT FOR TOP 12" OF SUBGRADE IS 95%. WHEN CLASS II BASE IS USED UNDER STREET PAVEMENT, CLASS II BASE SHALL ALSO BE PLACED UNDER CURB AND GUTTER.
2. NO FORMS SHALL BE PLACED UNTIL THE RELATIVE COMPACTION HAS BEEN TESTED AND APPROVED, UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.
3. ALL FORMWORK SHALL BE INSPECTED AND APPROVED PRIOR TO PLACEMENT OF CONCRETE.
4. THE CONTRACTOR SHALL USE 520-C-2500 CONCRETE UNLESS OTHERWISE APPROVED.
5. THE CONTRACTOR SHALL LOCATE EXPANSION AND WEAKENED PLANE JOINTS PER CITY STANDARD PLAN FOR SIDEWALKS.
6. IMMEDIATELY AFTER FINISHING OPERATIONS ARE COMPLETE, THE CONTRACTOR SHALL PROVIDE BROOM FINISH AND APPLY CURING COMPOUND.
7. THE CONTRACTOR SHALL PROTECT THE CONCRETE WORK FROM ALL TRAFFIC AND CONSTRUCTION EQUIPMENT FOR AT LEAST SEVEN DAYS UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.
8. FOR ABRUPT CURB TERMINATIONS, 3 FT. CURB TRANSITIONS SHALL BE PROVIDED, SEPARATED WITH A WEAKENED PLANE JOINT.

NOT TO SCALE



APPROVED BY:

Ricardo Sandoval 10.18.06
CITY ENGINEER DATE

RICARDO SANDOVAL

REVIEWED BY: *RS*

DATE OF LAST REVISION: _____

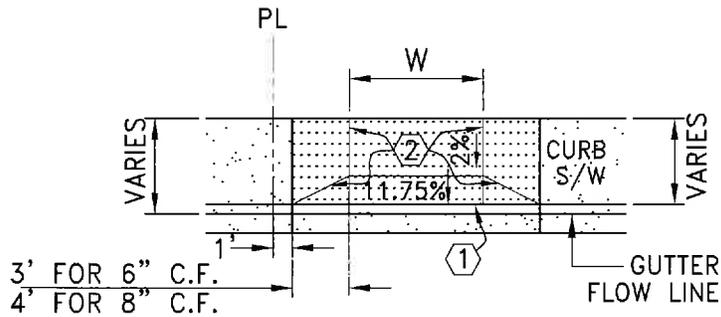
CITY OF FONTANA

CURB AND GUTTER

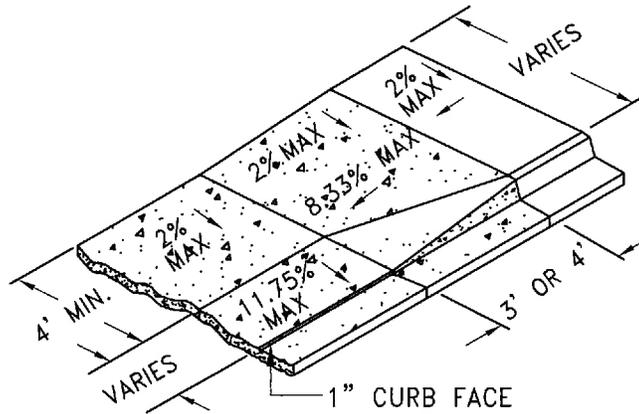
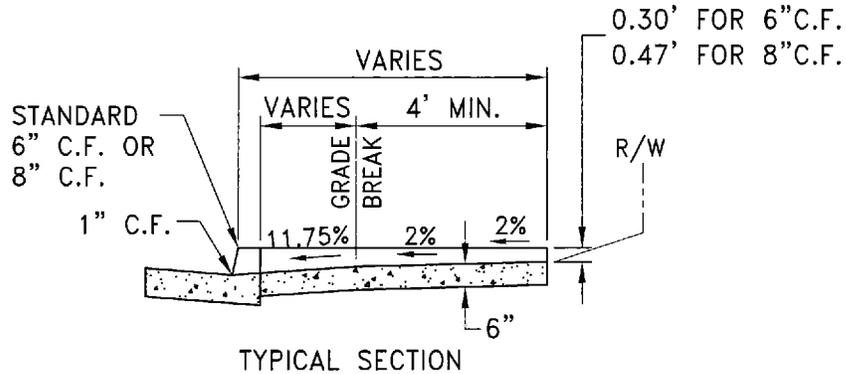
07/10/06

STD. PLAN NO. 1000

SHT 1 OF 1



DRIVEWAY APPROACH WITH CURB ADJACENT SIDEWALK



ISOMETRIC VIEW

 CONCRETE IN THE SHADED AREA SHALL BE AT LEAST 6" THICK.

- ① SCORE LINE OR COLD JOINT.
- ② GRADE BREAK LINES SHALL BE PRECISE AND STRAIGHT. SCREEDS AND/OR FALSE FORMS MUST BE USED TO ACHIEVE PRECISE CONSTRUCTION

NOT TO SCALE



APPROVED BY:  9.26.19
 CITY ENGINEER DATE
RICARDO SANDOVAL
 REVIEWED BY: 
 DATE OF LAST REVISION: 09/26/19

CITY OF FONTANA
RESIDENTIAL / COMMERCIAL / INDUSTRIAL DRIVEWAY APPROACH WITH CURB ADJACENT SIDEWALKS
 09/26/19
 STD. PLAN NO. **1001** SHT **1** OF **2**

DRIVEWAY APPROACH DIMENSIONS	
LAND USE & DRIVEWAY TYPE	W
SINGLE-FAMILY DWELLING OR DUPLEX	12 FT-24 FT MAX
DETACHED GARAGE LOCATED IN REAR YARD FOR SINGLE FAMILY DWELLING	12 FT
TWO-WAY TRUCK ACCESS FOR COMMERCIAL SITES	35 FT
TWO-WAY TRUCK ACCESS FOR INDUSTRIAL SITES	40 FT

NOTES:

1. A CONSTRUCTION PERMIT IS REQUIRED PRIOR TO CONSTRUCTION OF A DRIVEWAY APPROACH.
2. NO PORTION OF A DRIVEWAY APPROACH SHALL BE LOCATED WITHIN A CURB RETURN.
3. DRIVEWAY APPROACHES SERVING ADJOINING LOTS SHALL BE SEPARATED BY AT LEAST ONE FOOT OF FULL HEIGHT CURB.
4. WHEN A JOINT DRIVEWAY APPROACH IS PERMITTED, A RECORDED EASEMENT ALLOWING FOR MUTUAL ACCESS ON THE ADJOINING PROPERTIES IS REQUIRED.
5. THERE SHALL BE AL LEAST 20 FEET OF FULL HEIGHT CURB BETWEEN DRIVEWAYS SERVING THE SAME LOT.
6. CONCRETE SHALL BE CLASS 520-C-3250.
7. INSTALL ¼" EXPANSION JOINT FILLER AT LIMITS OF DRIVEWAY APPROACH.
8. WHEN A DRIVEWAY APPROACH IS TO JOIN AN ALLEY, THE DRIVEWAY APPROACH AND THE ALLEY SHALL BE CONSTRUCTED TO ALLOW FOR PROPER DRAINAGE.



APPROVED BY:

Ricardo Sandoval 9.26.19

CITY ENGINEER DATE
RICARDO SANDOVAL

REVIEWED BY: *JK*

DATE OF LAST REVISION: 09/26/19

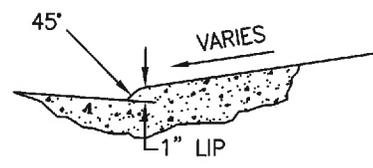
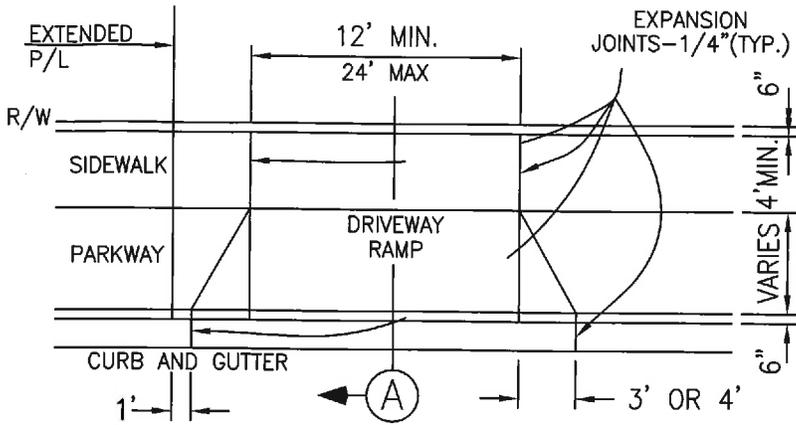
CITY OF FONTANA

**RESIDENTIAL / COMMERCIAL /
INDUSTRIAL DRIVEWAY
APPROACH WITH CURB
ADJACENT SIDEWALKS**

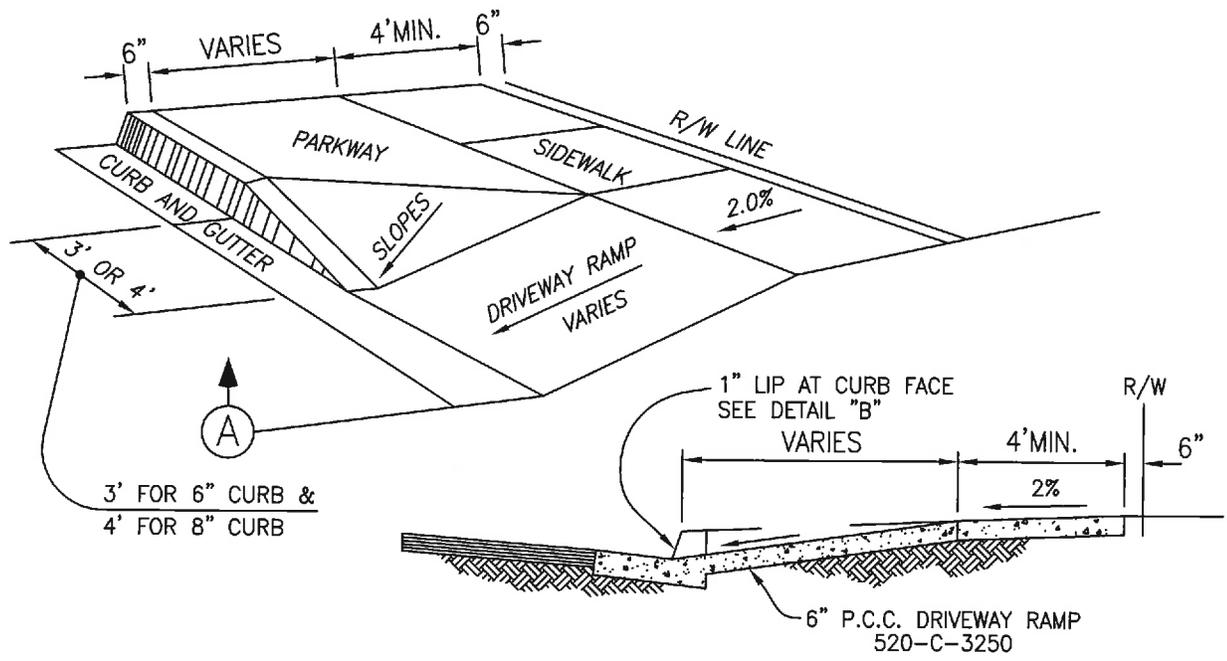
09/26/19

STD. PLAN NO. **1001**

SHT **2** OF **2**



DETAIL "B"



SECTION A-A

NOTES:

1. THERE SHALL BE AT LEAST 20' OF FULL HEIGHT CURB BETWEEN DRIVEWAYS SERVING THE SAME LOT.
2. DRIVEWAY APPROACHES SERVING ADJOINING LOTS SHALL BE SEPARATED BY AT LEAST ONE FOOT OF FULL HEIGHT CURB.
3. NO PORTION OF A DRIVEWAY SHALL EXTEND IN FRONT OF AN ADJOINING LOT.
4. CONTRACTOR SHALL USE 520-C-3250 CONCRETE FOR DRIVEWAYS.
5. 24' DRIVEWAY WIDTH IS FOR THREE CAR GARAGES, 12' AND 16' ARE FOR SINGLE AND TWO CAR GARAGES RESPECTIVELY.
6. CONTRACTOR SHALL PROVIDE HEAVY BROOM FINISH.
7. IMMEDIATELY AFTER FINISHING OPERATIONS ARE COMPLETE, CURING COMPOUND SHALL BE APPLIED.

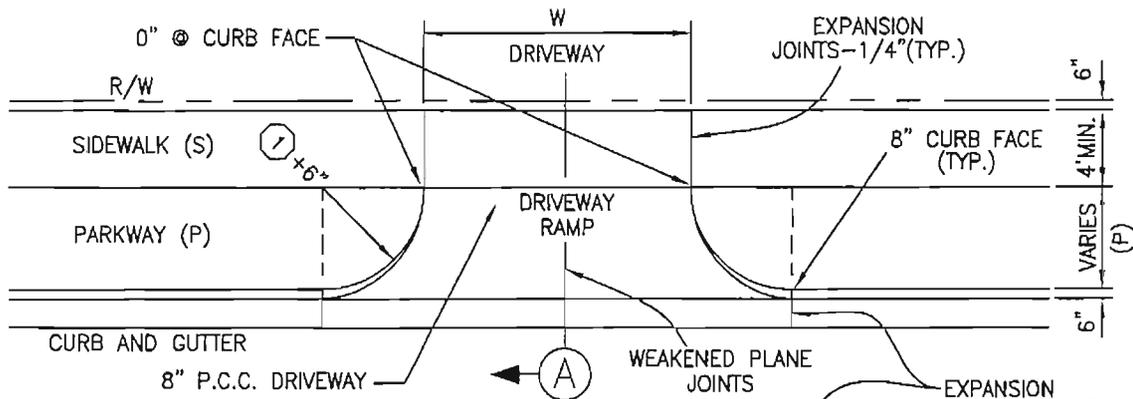
NOT TO SCALE



APPROVED BY:
Ricardo Sandoval 5.12.16
 CITY ENGINEER DATE
RICARDO SANDOVAL
 REVIEWED BY: *JK*
 DATE OF LAST REVISION: 05/12/16

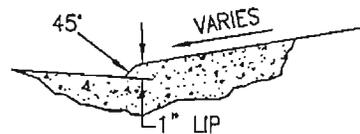
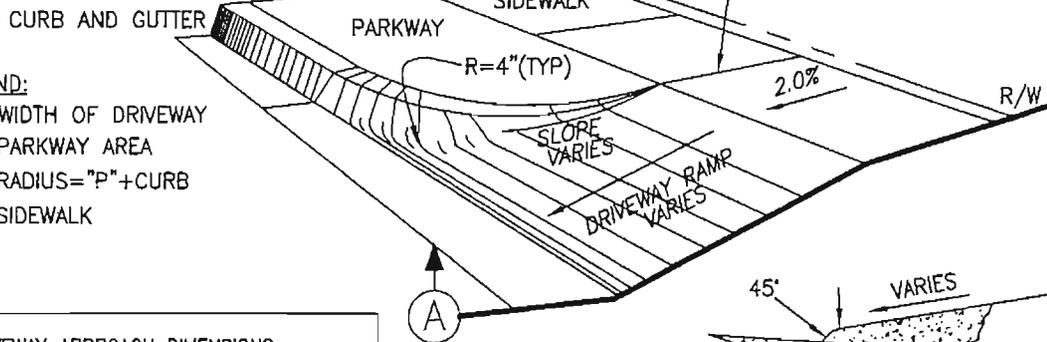
CITY OF FONTANA	
RESIDENTIAL / COMMERCIAL / INDUSTRIAL DRIVEWAY WITH PARKWAY SIDEWALKS	
STD. PLAN NO. 1002	SHT 1 OF 2

07/10/06



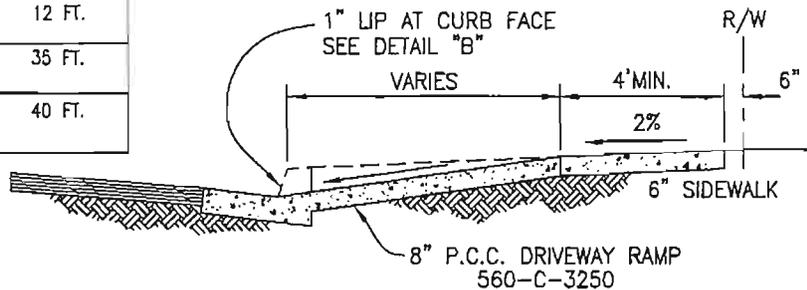
LEGEND:

- (W)=WIDTH OF DRIVEWAY
- (P)=PARKWAY AREA
- (1) RADIUS="P"+CURB
- (S)=SIDEWALK



DETAIL "B"

DRIVEWAY APPROACH DIMENSIONS	
LAND USE & DRIVEWAY TYPE	A
SINGLE-FAMILY DWELLING OR DUPLEX	12 FT. TO 24 FT. MAX.
DETACHED GARAGE LOCATED IN REAR YARD FOR SINGLE FAMILY DWELLING	12 FT.
TWO-WAY TRUCK ACCESS FOR COMMERCIAL SITES	35 FT.
TWO-WAY TRUCK ACCESS FOR INDUSTRIAL SITES	40 FT.



SECTION A-A

NOTES:

1. DRIVEWAY WIDTH SHALL PROVIDE FOR ADEQUATE TRUCK TURNING MOVEMENT.
2. NO PORTION OF A DRIVEWAY SHALL EXTEND IN FRONT OF AN ADJOINING LOT.
3. PROVIDE BROOM FINISH ON RAMP AND SIDEWALK.
4. IMMEDIATELY AFTER FINISHING OPERATIONS ARE COMPLETE, CURING COMPOUND SHALL BE APPLIED.

NOT TO SCALE



APPROVED BY:

Ricardo Sandoval
 CITY ENGINEER
RICARDO SANDOVAL

DATE

REVIEWED BY: *DB*

DATE OF LAST REVISION: _____

CITY OF FONTANA

**RESIDENTIAL / COMMERCIAL /
 INDUSTRIAL DRIVEWAY
 WITH PARKWAY SIDEWALKS**

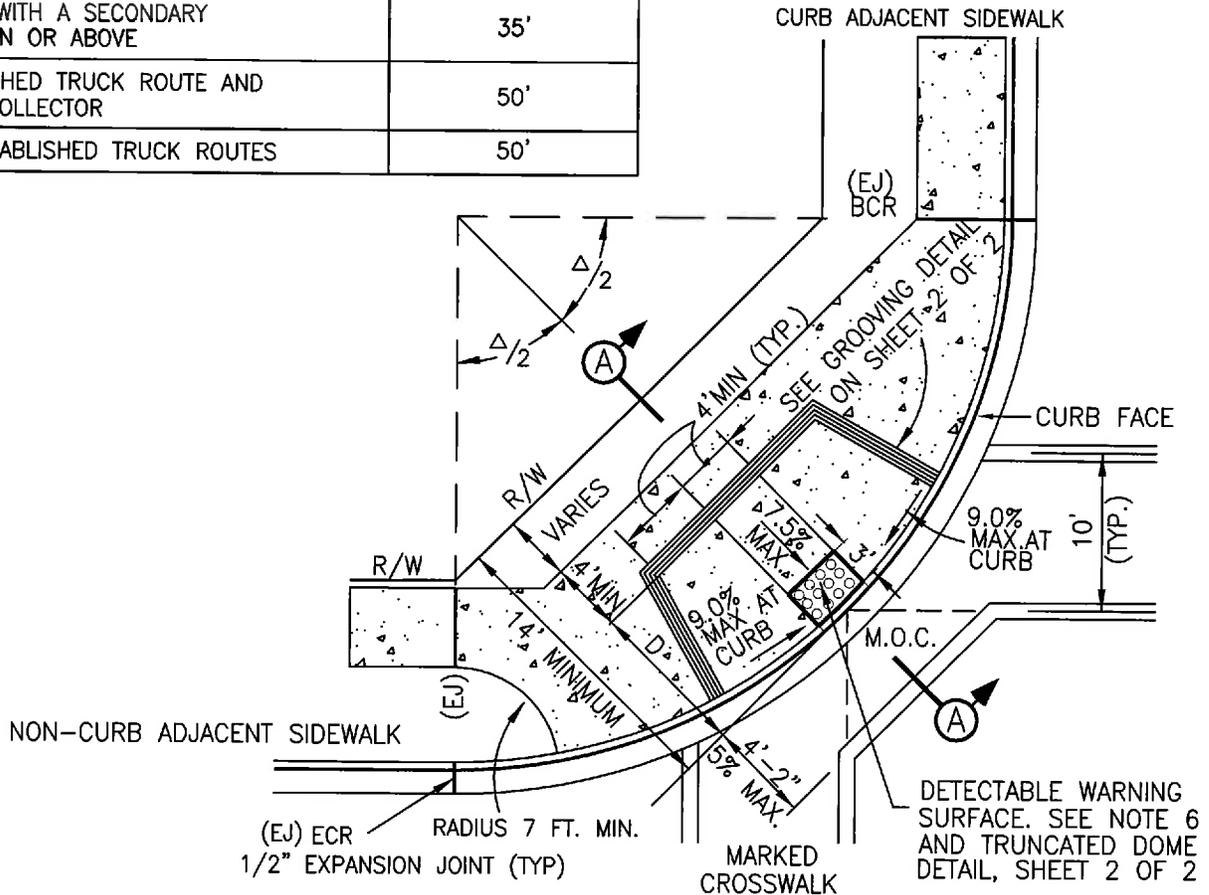
07/10/06

STD. PLAN NO. **1002**

SHT **2** OF **2**

CURB RETURN RADII

INTERSECTION OF:	MINIMUM RADIUS:
LOCAL STREET AND LOCAL STREET	25'
LOCAL STREET AND COLLECTOR STREET	30'
COLLECTOR STREET AND COLLECTOR STREET	35'
ANY STREET WITH A SECONDARY CLASSIFICATION OR ABOVE	35'
ANY ESTABLISHED TRUCK ROUTE AND INDUSTRIAL COLLECTOR	50'
ANY TWO ESTABLISHED TRUCK ROUTES	50'



NOTES:

1. RAMP SHALL HAVE A 12" WIDE BORDER ON FLAT SURFACE WITH 1/4" GROOVES APPROXIMATELY 3/4" O.C. AS SHOWN IN GROOVING DETAIL.
2. RAMP SHALL HAVE TRANSVERSE BROOM FINISH TEXTURE ROUGHER THAN THE SURROUNDING SIDEWALK.
3. RAMP GRADES SHALL BE STAKED FOR INSPECTION PRIOR TO PLACEMENT OF CONCRETE.
4. MINIMUM CURB RETURN RADII SHALL BE PER TABLE SHOWN ABOVE UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.
5. CONCRETE THICKNESS SHALL BE 6 INCHES FROM ECR TO BCR UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.
6. THE CURB RAMP DETECTABLE WARNING SURFACE MUST BE PREFABRICATED SURFACE, RIGID, COLORFAST AND UV STABLE, YELLOW COLOR COMPLYING WITH FEDERAL STANDARD 595B, COLOR NUMBER 33538 "ADA REPLACEABLE (WET SET) COMPOSITE TACTILE UNITS" AS MANUFACTURED BY ADA SOLUTIONS, ARMOR-TILE, OR EQUAL TO COMPLY WITH STATE AND FEDERAL REQUIREMENTS. THE EDGE OF THE DETECTABLE WARNING SURFACE ADJACENT TO THE STREET SHALL BE BETWEEN 6" AND 8" FROM THE GUTTER FLOW LINE.
7. THE MAXIMUM SLOPE OF THE ROAD SURFACE IMMEDIATELY ADJACENT TO THE CURB RAMP SHALL NOT EXCEED 5 PERCENT WITHIN 4' OF THE RAMP.
8. FOR DIMENSIONS NOT SHOWN, REFER TO THE LATEST CALTRANS ADA STANDARD PLAN

NOT TO SCALE



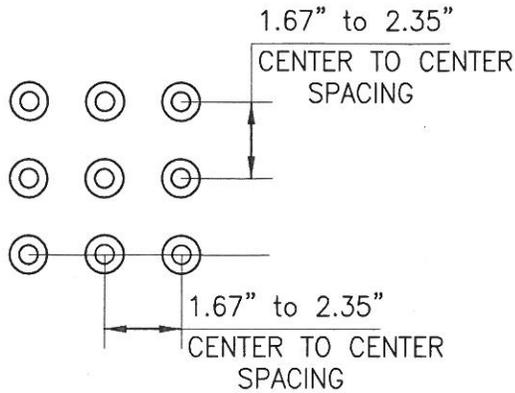
APPROVED BY:
Ricardo Sandoval 5-12-16
CITY ENGINEER **DATE**
RICARDO SANDOVAL
REVIEWED BY: *JK*
DATE OF LAST REVISION: 5/12/16

CITY OF FONTANA

**CURB RETURN
ACCESS RAMP**

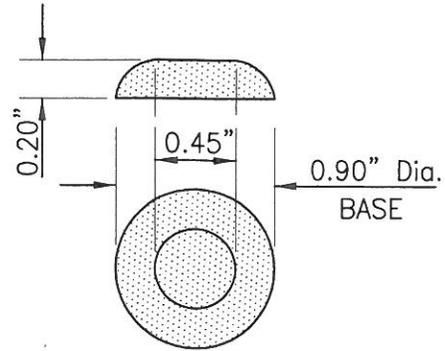
12/12/11

STD. PLAN NO. 1003 SHT 1 OF 2



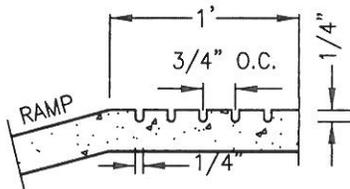
**TRUNCATED
DOME PATTERN**

DETECTABLE WARNING SYSTEM
SEE NOTE 6

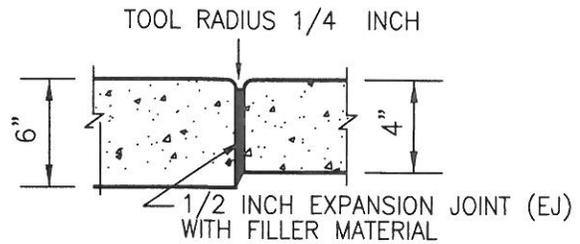


**TRUNCATED
DOME DETAIL**

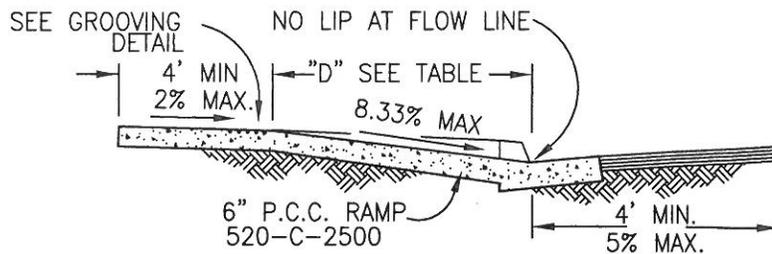
RAISED TRUNCATED DOME



GROOVING DETAIL
N.T.S.



EXPANSION JOINT DETAIL



SECTION "A-A"
N.T.S.
TABLE "D"

DEPTH OF RAMP "D"	
	8" CURB FACE
"D"	9'- 9" MIN.

NOT TO SCALE



APPROVED BY:
[Signature] 12.13.11
CITY ENGINEER DATE
RICARDO SANDOVAL
REVIEWED BY: *[Signature]*
DATE OF LAST REVISION: 12/12/11

CITY OF FONTANA

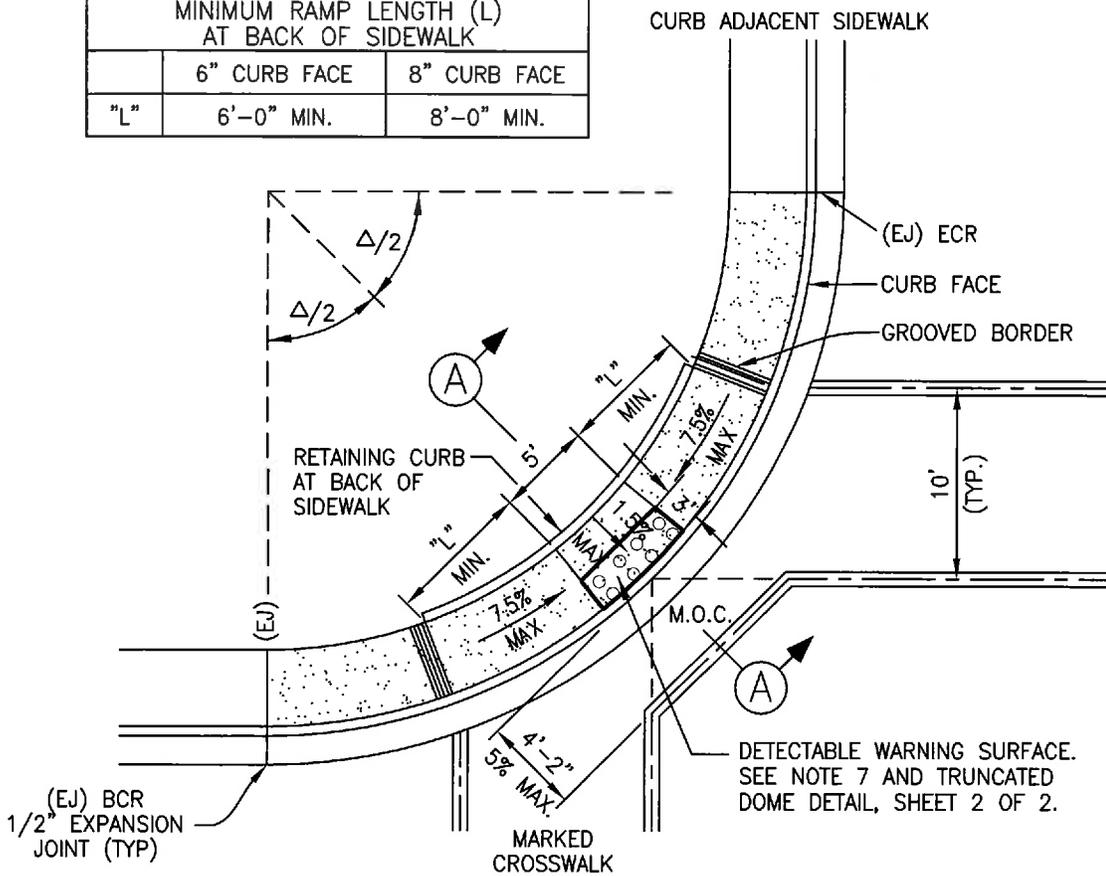
**CURB RETURN
ACCESS RAMP**

12/12/11

STD. PLAN NO. 1003

SHT 2 OF 2

MINIMUM RAMP LENGTH (L) AT BACK OF SIDEWALK		
	6" CURB FACE	8" CURB FACE
"L"	6'-0" MIN.	8'-0" MIN.



NOTES:

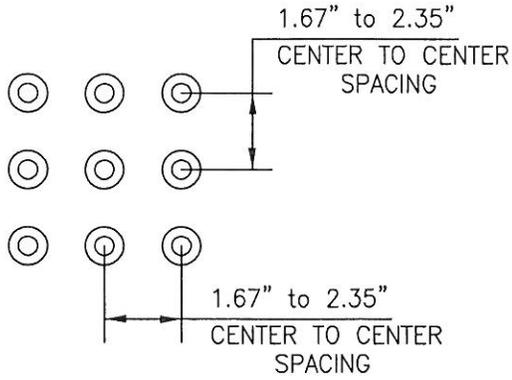
1. MODIFIED ACCESS RAMP SHALL BE USED WHEN RIGHT OF WAY IS RESTRICTED.
2. ALL OBSTRUCTIONS SHALL BE REMOVED OR RELOCATED TO PROVIDE FOR A MINIMUM FOUR (4) FOOT CLEARANCE.
3. RAMP SHALL HAVE A 12" WIDE BORDER ON FLAT SURFACE WITH 1/4" GROOVES APPROXIMATELY 3/4" ON CENTER AS SHOWN IN GROOVING DETAIL.
4. RAMP SHALL HAVE TRANSVERSE BROOM FINISH TEXTURE ROUGHER THAN THE SURROUNDING SIDEWALK.
5. RAMP SHALL BE STAKED FOR INSPECTION PURPOSES PRIOR TO POURING CONCRETE.
6. CONCRETE THICKNESS SHALL BE 6 INCHES FROM ECR TO BCR.
7. THE CURB RAMP DETECTABLE WARNING SURFACE MUST BE PREFABRICATED SURFACE, RIGID, COLORFAST AND UV STABLE, YELLOW COLOR COMPLYING WITH FEDERAL STANDARD 595B, COLOR NUMBER 33538, "ADA REPLACEABLE (WET SET) COMPOSITE TACTILE UNITS" AS MANUFACTURED BY ADA SOLUTIONS, ARMOR-TILE, OR EQUAL TO COMPLY WITH STATE AND FEDERAL REQUIREMENTS. THE EDGE OF THE DETECTABLE WARNING SURFACE ADJACENT TO THE STREET SHALL BE BETWEEN 6" AND 8" FROM THE GUTTER FLOW LINE.
8. THE MAXIMUM SLOPE OF THE ROAD IMMEDIATELY ADJACENT TO THE CURB RAMP SHALL NOT EXCEED 5 PERCENT WITHIN 4' OF THE RAMP.
9. FOR DIMENSIONS NOT SHOWN, REFER TO THE LATEST CALTRANS ADA STANDARD PLANS.

NOT TO SCALE



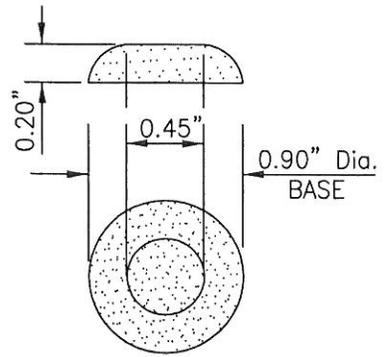
APPROVED BY: *Ricardo Sandoval* 5.12.16
 CITY ENGINEER DATE
RICARDO SANDOVAL
 REVIEWED BY: *JK*
 DATE OF LAST REVISION **5/12/2016**

CITY OF FONTANA	
MODIFIED CURB RETURN ACCESS RAMP	
09/14/10	
STD. PLAN NO. 1004	SHT 1 OF 2



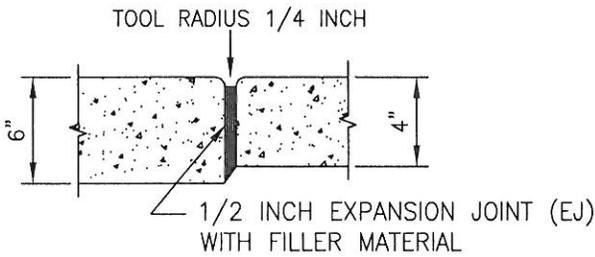
TRUNCATED DOME PATTERN

DETECTABLE WARNING SYSTEM
SEE NOTE 7

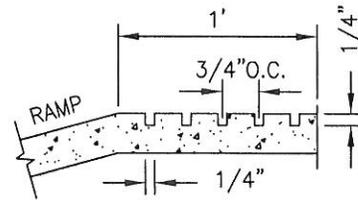


TRUNCATED DOME DETAIL

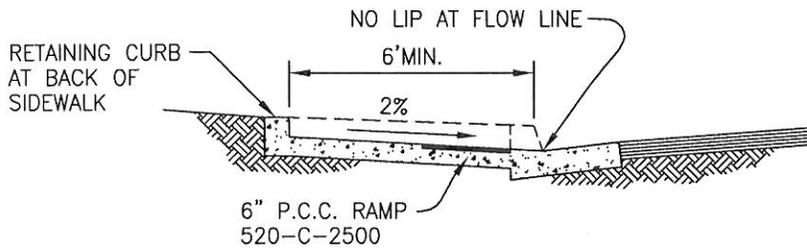
RAISED TRUNCATED DOME



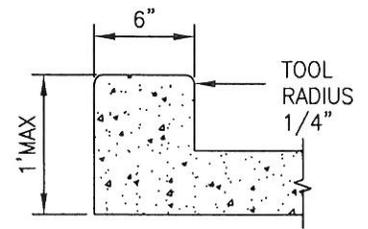
EXPANSION JOINT DETAIL



GROOVING DETAIL



SECTION A-A



RETAINING CURB

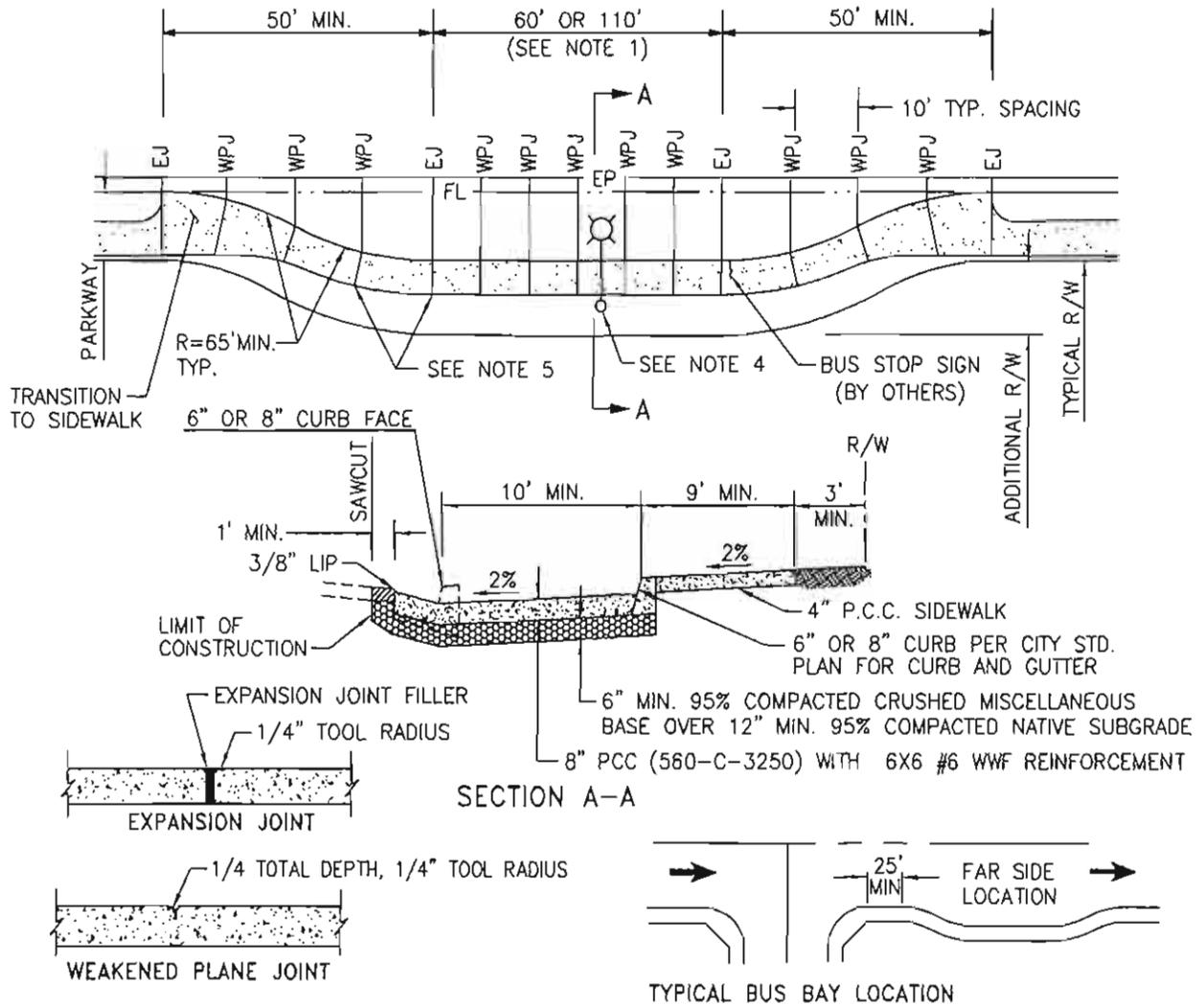
NOT TO SCALE



APPROVED BY: *[Signature]* 9-14-10
 CITY ENGINEER DATE
RICARDO SANDOVAL
 REVIEWED BY: *[Signature]*
 REVISION NUMBER: 1

CITY OF FONTANA	
MODIFIED CURB RETURN ACCESS RAMP	
STD. PLAN NO. 1004	SHT 2 OF 2

09/14/10



NOTES:

1. LENGTH OF STORAGE DETERMINED BY SINGLE OR DOUBLE BUS STOP AS SPECIFIED BY SERVING BUS COMPANY AND APPROVED BY THE CITY ENGINEER.
2. IF BENCH/SHELTER IS PROPOSED, 4 FEET MINIMUM OF SIDEWALK CLEARANCE IS REQUIRED.
3. FAR SIDE BUS BAY LOCATION IS PREFERRED, PER ABOVE DETAIL.
4. ADDITIONAL STREET LIGHTING MAY BE REQUIRED AS DETERMINED BY THE CITY ENGINEER.
5. CONSTRUCT EXPANSION JOINTS (EJ) AND WEAKENED PLANE JOINTS (WPJ) PER ABOVE DETAIL, OR AS DIRECTED BY THE CITY ENGINEER.

NOT TO SCALE



APPROVED BY:

Ricardo Sandoval
 CITY ENGINEER
RICARDO SANDOVAL

DATE

REVIEWED BY: _____

DATE OF LAST REVISION: 12/03/07

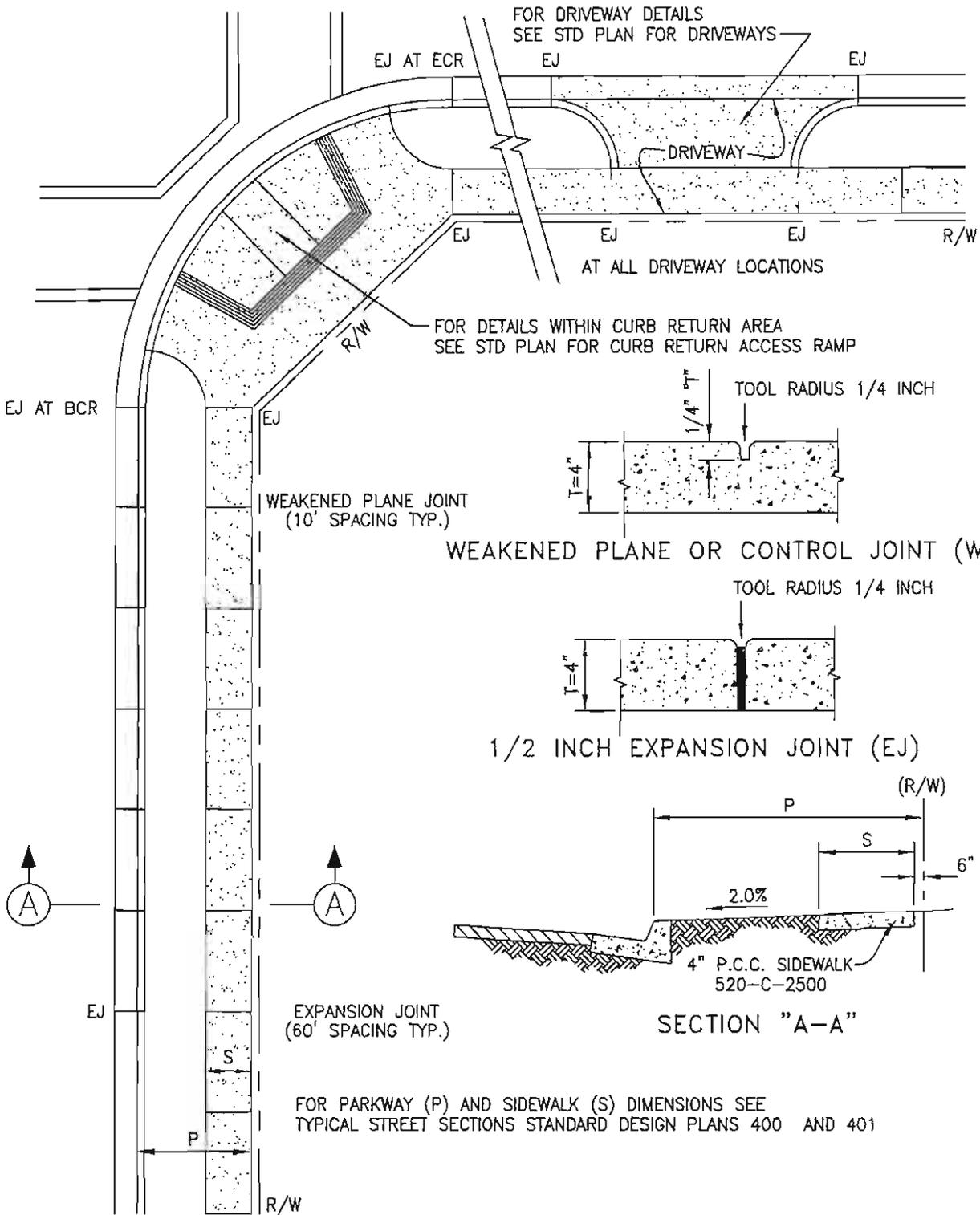
CITY OF FONTANA

**STANDARD
 BUS BAY**

07/10/06

STD. PLAN NO. **1005**

SHT **1** OF **1**



NOT TO SCALE



APPROVED BY: *[Signature]* 10.18.06
 CITY ENGINEER DATE
RICARDO SANDOVAL
 REVIEWED BY: *Dh*
 DATE OF LAST REVISION: _____

CITY OF FONTANA

SIDEWALKS

07/10/06

STD. PLAN NO. 1006

SHT 1 OF 2

SIDEWALK CONSTRUCTION NOTES

1. ALL SIDEWALKS SHALL BE CONSTRUCTED AS SPECIFIED IN THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION (GREEN BOOK), UNLESS OTHERWISE NOTED.
2. ALL SUBGRADE SHALL BE CONSTRUCTED TO GRADE AND CROSS SECTION PER CITY STANDARD PLANS FOR APPLICABLE STREET SECTION. TOLERANCES FOR SIDEALKS AND CURBS AND GUTTERS SHALL BE PER THE GREEN BOOK.
3. SUBGRADE SHALL BE SCARIFIED AND COMPACTED TO A MINIMUM DEPTH OF 12 INCHES. 90% COMPACTION IS REQUIRED BEHIND THE CURB AND IN PARKWAY AREA. AFTER COMPACTION, THE SUBGRADE SHALL BE FREE OF UNSUITABLE MATERIALS.
4. ALL CONCRETE SHALL BE 520-C-2500 UNLESS OTHERWISE SPECIFIED.
5. CONCRETE SHALL BE OF THE SPECIFIED SLUMP AND WITHIN THE REQUIRED WATER/CEMENT RATIO.
6. THE CONTRACTOR SHALL VIBRATE ALL CONCRETE ADEQUATELY TO INSURE CONSOLIDATION FREE OF VOIDS.
7. ALL SIDEWALKS, CURBS AND CURB AND GUTTERS SHALL HAVE WEAKENED PLANE JOINTS SPACED AT 10 FOOT MAXIMUM INTERVALS. EXPANSION JOINTS SHALL BE PLACED AT 60 FOOT INTERVALS UTILIZING 1/2 INCH EXPANSION JOINT MATERIAL.
8. A NORMAL EXPANSION JOINT MAY BE OMITTED IF IT FALLS WITHIN 10 FEET OF AN ADDITIONAL EXPANSION JOINT.
9. WEAKENED PLANE JOINTS SHALL BE 1/4 OF THE DEPTH OF THE CONCRETE BUT NOT LESS THAN 1 INCH. CURBS AND GUTTERS SHALL HAVE WEAKENED PLANE JOINTS OF 2 INCH MINIMUM DEPTH.
10. TRANSIT MIXED CONCRETE DELIVERY TICKETS SHALL BE PROVIDED TO THE INSPECTOR PRIOR TO PLACING CONCRETE TO INSURE THAT THE CONCRETE MEETS SPECIFICATIONS.
11. ALL SURFACES SHALL BE TRUE AND STRAIGHT AND OF UNIFORM WIDTH, FREE OF HUMPS, SAGS, IRREGULARITIES AND IMPERFECTIONS. UNIFORM SURFACES SHALL NOT VARY MORE THAN 0.01 FOOT WHEN MEASURED WITH A 10 FOOT STRAIGHT EDGE.
12. ALL SURFACES SHALL RECEIVE A BROOM FINISH.
13. UPON COMPLETION OF FINISHING OPERATIONS ALL SURFACES SHALL BE SPRAYED WITH CURING COMPOUND.
14. CONCRETE REPAIRS SHALL BE MADE BY SAW CUTTING AND REMOVING THE ENTIRE UNIT BETWEEN WEAKENED PLANE JOINTS.
15. SIDEWALKS WITHIN DRIVEWAYS SHALL BE CONSTRUCTED PER CITY STANDARD PLANS FOR DRIVEWAYS.

NOT TO SCALE



APPROVED BY: *[Signature]* 10.18.06
 CITY ENGINEER DATE
RICARDO SANDOVAL
 REVIEWED BY: *[Signature]*
 DATE OF LAST REVISION: _____

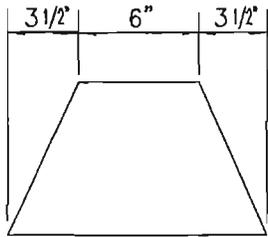
CITY OF FONTANA

SIDEWALKS

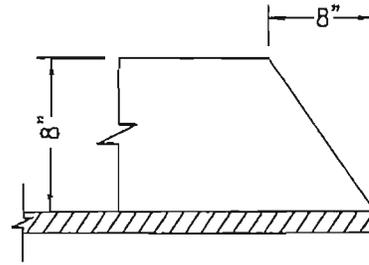
07/10/06

STD. PLAN NO. **1006**

SHT **2** OF **2**

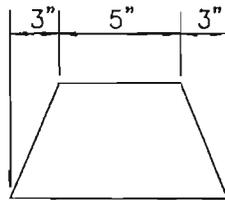


CROSS - SECTION

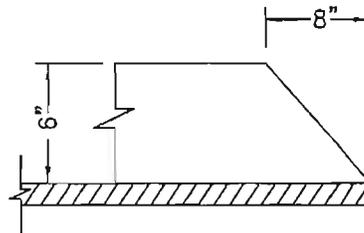


END TREATMENT

8" DIKE



CROSS - SECTION



END TREATMENT

6" DIKE

NOTES:

1. DIKE SHALL BE CONSTRUCTED OF TYPE AR-8000 OR PG-64-10 ASPHALT CONCRETE
2. TACT COAT BINDER SS1H SHALL BE PLACED ON EXISTING ASPHALT PAVING PRIOR TO THE INSTALLATION OF THE DIKE
3. DEPRESS DIKE AT DRIVEWAYS (3/4" C.F.) AND TERMINATE (0" C.F.) AT CROSSWALKS PER PLAN AND AS DIRECTED BY THE ENGINEER.
4. DIKE SHALL BE CONSTRUCTED PER THIS DETAIL AND CALTRANS STANDARD PLAN A87, LATEST EDITION

NOT TO SCALE



APPROVED BY:
Ricardo Sandoval
 10-18-06
 CITY ENGINEER DATE
RICARDO SANDOVAL
 DRAWN BY: _____
 DATE OF LAST REVISION: DB

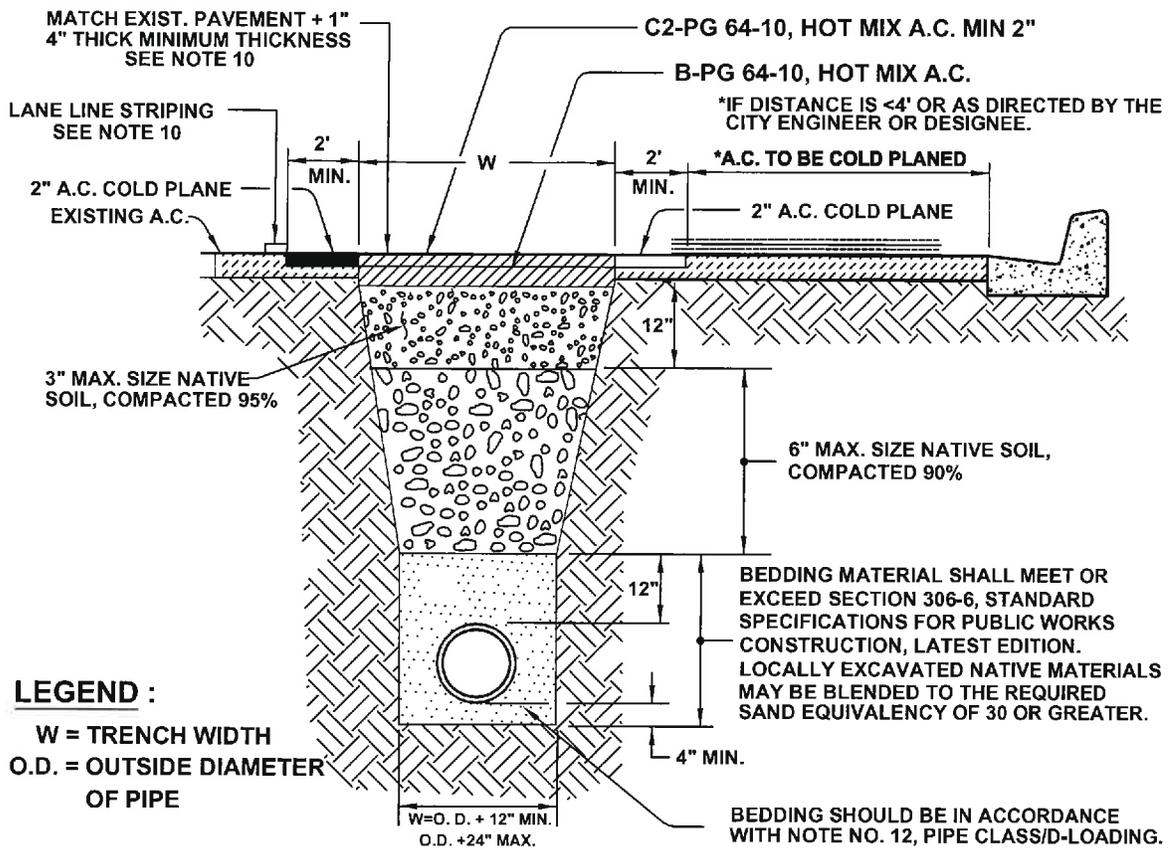
CITY OF FONTANA

ASPHALT CONCRETE
 DIKE

07/10/06

STD. PLAN NO. 1007

SHT 1 OF 1



GENERAL NOTES :

1. ALL EXCAVATION WITHIN THE CITY OF FONTANA RIGHT-OF-WAY REQUIRES AN EXCAVATION PERMIT FROM THE ENGINEERING DIVISION.
2. UNDERGROUND SERVICE ALERT SHALL BE NOTIFIED 2 WORKING DAYS PRIOR TO START OF WORK 1-800-422-4133
3. ALL PUBLIC WORKS INSPECTION REQUESTS WILL BE MADE 2 WORKING DAYS AHEAD OF SCHEDULED WORK.
4. COMPACTION OF BACKFILL SHALL BE VERIFIED BY REGISTERED GEOTECHNICAL ENGINEER AND A COMPACTION REPORT SUBMITTED TO THE CITY FOR APPROVAL PRIOR TO THE PLACING OF THE PERMANENT PAVEMENT. IN LIEU OF COMPACTION TESTING CONTRACTOR MAY USE 2-SAC PCC SLURRY.
5. ALL EXCAVATION SHALL BE MADE, PROTECTED AND SUPPORTED AS REQUIRED FOR SAFETY AND IN A MANNER SET FORTH IN OPERATIONS, RULES, ORDERS, AND REGULATIONS PRESCRIBED BY THE CALIFORNIA DIVISION OF INDUSTRIAL SAFETY. A COPY OF THE CAL OSHA EXCAVATION PERMIT, IF APPLICABLE, SHALL BE FURNISHED TO THE PROJECT INSPECTION PRIOR TO BEGINNING THE WORK. STEEL PLATE BRIDGING SHALL BE IN ACCORDANCE WITH CITY STD. NO. 1009.
6. PERMANENT PAVEMENT SHALL BE REPLACED WITHIN 30 DAYS OF EXCAVATION, A MINIMUM 2-INCH THICKNESS OF TEMPORARY ASPHALT PAVING SHALL BE PLACED WITHIN THE TRENCH AREA UNTIL PERMANENT REPAIR IS COMPLETED OR AS DIRECTED BY THE CITY ENGINEER OR DESIGNEE. THE TEMPORARY ASPHALT PAVING SHALL BE FLUSH WITH EXISTING PAVEMENT.
THE TEMPORARY PAVING SHALL BE PLACED AND COMPACTED IN SUCH A MANNER AS TO PROVIDE A SAFE AND SMOOTH TRAVELED SURFACE. PERMITTEE SHALL MAINTAIN THE TEMPORARY PAVEMENT IN A SAFE AND SMOOTH CONDITION UNTIL PERMANENT PAVING IS IN PLACE.



APPROVED BY: *Ricardo Sandoval* 8.7.19
 CITY ENGINEER DATE
 RICARDO SANDOVAL
 REVIEWED BY: *JL*
 DATE OF LAST REVISION: 8/7/19

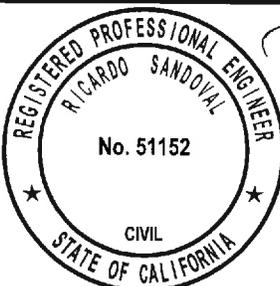
CITY OF FONTANA	
STANDARD PAVING AND TRENCH REPAIR	
STD. PLAN NO. 1008	SHT 1 OF 2

GENERAL NOTES : Continued

7. PRIOR TO PLACEMENT OF PERMANENT PAVING, EXISTING PAVEMENT SHALL BE CUT TO A NEAT STRAIGHT LINE. ALL PAVEMENT JOINTS OR CRACKS WITHIN 2 FEET OF TRENCH IN ALL DIRECTIONS, OR AS DIRECTED BY THE CITY ENGINEER OR DESIGNEE, SHALL BE REMOVED AND REPLACED.
8. ALL EDGES OF EXISTING PAVEMENT BEING JOINED AND SURFACE BEING OVERLAID SHALL RECEIVE A TACK COAT OF ASPHALT EMULSION.
9. ANY STREET PAVED OR RESURFACED IN THE PREVIOUS 36 MONTHS SHALL BE SUBJECT TO SPECIAL PAVING REQUIREMENTS.
10. ANY TRENCH EXTENDING FROM THE CURB INTO THE PARKING LANE, SHALL REQUIRE A COMPLETE GRIND AND OVERLAY OF THE PARKING LANE. ALL TRENCHES EXTENDING LONGITUDINALLY IN THE DRIVING LANE SHALL REQUIRE THAT THE ENTIRE LANE BE COLD MILLED AND OVERLAID. ALL TRENCHES EXTENDING INTO THE TRAVELED LANE TRANSVERSELY WILL REQUIRE THE ENTIRE LENGTH OF THE TRENCH UP TO THE NEAREST LANE LINE BE COLD MILLED AND OVERLAID 10 FEET IN BOTH DIRECTIONS FROM THE CENTERLINE OF THE TRENCH.
11. IF TRENCH FAILURE SHOULD OCCUR, THE PERMITEE/DEVELOPER WILL BE NOTIFIED OF SUCH DEFICIENCIES AND DIRECTED TO REMOVE, REPLACE, REMEDY THIS WORK. UPON FAILURE OF THE CONTRACTOR TO PROMPTLY COMPLY AND UNDER ORDER OF THE CITY ENGINEER, TRENCH SHALL BE REMEDIED, REMOVED, REPLACED AT PERMITEE/DEVELOPERS SOLE EXPENSE.
12. BEDDING MATERIAL SHALL BE SAND, GRAVEL, CRUSHED MISCELLANEOUS BASE OR NATIVE FREE-DRAINING GRANULAR MATERIAL HAVING A SAND EQUIVALENT OF NOT LESS THAN 30, AND SHALL HAVE A PERCENTAGE COMPOSITION BY WEIGHT WHICH CONFORMS TO THE FOLLOWING GRADING:

<u>SIEVE SIZES</u>	<u>% PASSING SIEVES</u>
1 - 1/2"	-
1"	-
3/4"	100
1/2"	90 - 100
3/8"	20 - 60
No. 4	0 - 15
No. 8	0 - 5

13. THE CONTRACTOR/PERMITEE SHALL BE REQUIRED TO FOLLOW THE WORK AREA TRAFFIC CONTROL HANDBOOK ("WATCH" MANUAL) OR AS DIRECTED BY THE CITY TRAFFIC ENGINEER OR DESIGNEE.
14. FULL STREET CLOSURES REQUIRE CITY TRAFFIC ENGINEER APPROVAL AND REQUEST FOR SUCH CLOSURES SHALL BE SUBMITTED 20 DAYS IN ADVANCE OF CLOSURE.
15. THE WORKING HOURS ON ARTERIAL AND MAJOR STREETS, SCHOOLS, HOSPITALS, AND FREEWAY RAMPS SHALL BE SUBJECT TO APPROVAL BY CITY TRAFFIC ENGINEER'S OFFICE.
16. ALL STRIPING AND MARKINGS DAMAGED DURING CONSTRUCTION SHALL BE REPLACED IN KIND AS DIRECTED BY THE CITY ENGINEER OR DESIGNEE.
17. ALL POTHoles / SERVICE CUTS / ETC. SHALL BE BACK FILLED WITH A 2-SACK SLURRY OR AS SPECIFIED BY THE CITY ENGINEER OR DESIGNEE.
18. SOIL-STERILANT IS REQUIRED PRIOR TO PLACEMENT OF PERMANENT PAVING.
19. CONTRACTOR/PERMITTER SHALL CLEAN UP TRACKING BY ANY MEANS NECESSARY



APPROVED BY: *[Signature]* 1-3-18
 CITY ENGINEER DATE
RICARDO SANDOVAL
 REVIEWED BY: *JK*
 DATE OF LAST REVISION: 01/03/18

CITY OF FONTANA

STANDARD
PAVING AND TRENCH REPAIR

STD. PLAN NO. **1008** | SHT 2 OF 2

PLATE BRIDGING

WIDTH OF TRENCH

1.0 FOOT TO 3 FOOT
4.0 FEET

MINIMUM PLATE THICKNESS

----- 1 INCH
----- 1-1/4 INCH

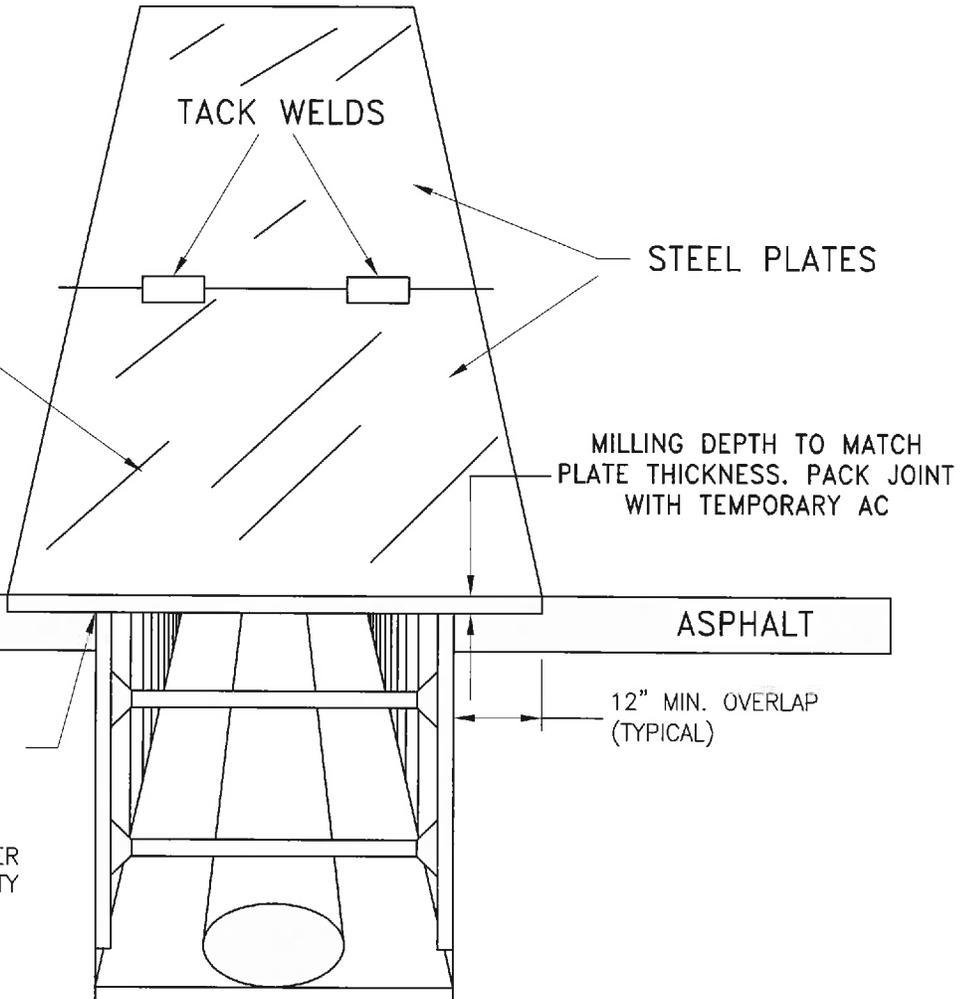
FOR SPANS GREATER THAN 4 FEET, MINIMUM PLATE THICKNESS SHALL BE IN ACCORDANCE WITH THE MANUFACTURERS ENGINEERED SPEC SHEET.

NOTE:

TAPERED WOOD CLEATS ARE USUALLY USED UNDER PLATES TO KEEP PLATES FROM ROCKING.

SURFACE OF STEEL PLATES SHALL BE ROUGHENED, TAPED OR COATED TO PROVIDE A NON-SKID SURFACE.

BRIDGING SHALL BE SECURED AGAINST MOVEMENT BY USING HOLDING DEVICES SUCH AS ADJUSTABLE CLEATS, ANGLES, BOLTS, TACK WELDING OR OTHER DEVICES, APPROVED BY THE CITY ENGINEER.



NOTE:

- * TRENCH WALLS AND ADJACENT SOIL SHALL BE SUFFICIENTLY STABLE FOR THE USE OF THE ABOVE PLATE.
- * CONTRACTOR/PERMITTEE SHALL BE RESPONSIBLE AT ALL TIMES FOR MAINTAINANCE OF THE PLATES FOR THE SAFE OPERATION OF TRAFFIC AND PEDESTRIAN SAFETY.
- * CONTRACTOR/PERMITTEE SHALL COLDMILL TO RECESS PLATES
- * MINIMUM PLATE DIMENSION SHALL BE 8' X 10'

NOT TO SCALE



APPROVED BY: *Ricardo Sandoval* . 8.7.19
 CITY ENGINEER DATE
RICARDO SANDOVAL
 REVIEWED BY: *JK*
 DATE OF LAST REVISION: 8/7/2019

CITY OF FONTANA	
PLATE BRIDGING	
07/10/06	
STD. PLAN NO. 1009	SHT 1 OF 1